

TWO STOREY SIDE AND ROOF EXTENSION WITH
SINGLE STOREY REAR EXTENSION

OAKDENE, OALANDS ROAD, BROMLEY BR1 3SL

1701 DESIGN AND ACCESS STATEMENT

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12 JUNE 2017



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1.0 EXECUTIVE SUMMARY

This proposal is for the side and rearwards extension of a detached 1930's property on Oaklands Road, Bromley.

The proposal sits within the LB Bromley side space policy and the relationship with adjacent properties is further helped by the presence of access driveways to either side of the site, giving access to the two significant late Victorian villas adjacent, which have since been converted into multiple flats.

These properties are significantly larger and higher than the application property. We have therefore made additional changes to the roof form, which is currently a crown roof, with a section of flat roof at centre.

The application proposal extends the front and rear roof slopes to meet at a new ridge, forming small gables at the side of this roof form.

The rear extension is a single storey flat roofed structure, extending 5.0m from the existing rear line of the application property. The existing ground floor habitable rooms of the adjacent properties will be largely unaffected by this proposal, which although extending further than might be typically acceptable, remains behind the rear line of the adjacent property at No 22 Oaklands Road and at a significant distance from; and directly to the north (thereby having zero impact on access to sunlight) of No 20.

The application sits outside any Conservation Area, however given the changes to the roof-form and the 5m projection of the rear extension, we feel this application might be better appreciated when rear in conjunction with this statement.

2.0 SITE ANALYSIS

2.1 AREA APPRAISAL

Oaklands Road is a varied collection of residential properties. There is a small number of significant Victorian villas present, each of differing design. Most of these, including the two immediately adjacent to the application address (at 20 and 22 Oaklands Road) have been converted to flats. Oakdene appears to have been constructed in the grounds of No 22.

There are also a number of post-war houses and purpose built flatted developments on Oaklands Road. These are extremely varied and although there might be some commonality in each individual section, Oaklands Road extremely architecturally diverse.

Oakdene is one of only two relatively modest 1930's detached properties. The other is at the other end of Oaklands Road, although this appears as though it might alternatively date from the 1950's. This is different in design to Oakdene.

2.2 LOCAL CONTEXT

Oakdene is a two storey detached 1930's property with a two storey hipped bay and a single storey garage with hipped roof. The main roof is crowned and is significantly lower than the two adjacent Victorian villas.

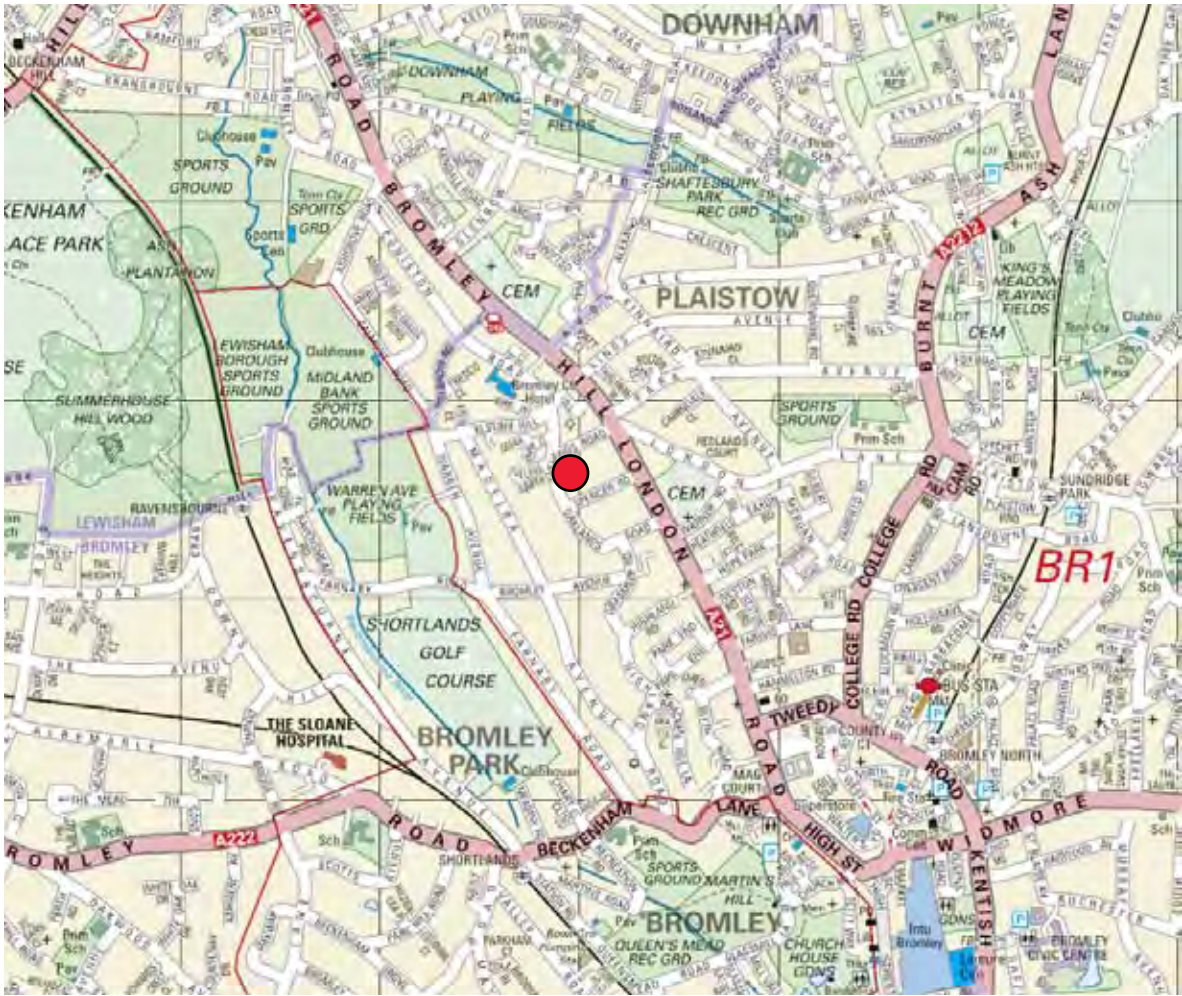
The application site is separated from the adjacent buildings by a clear passing space on both sides within the site boundary as well as broad driveways access the parking to the rear of the converted flats at No's 20 and 22. There is therefore an existing separation of 4.075m and 7.500m between the application property and No's 22 and 20 respectively.



OAKDENE ALONSIDE NO 22 OAKLANDS RD



OAKDENE ALONSIDE NO 20 OAKLANDS RD



SITE LOCATION PLAN



SITE AERIAL PHOTO

2.3 EXISTING SITE PHOTOGRAPHS



- 1. Oakdene, front street elevation.
- 2. Oakdene, rear elevation.
- 3. Oakdene alongside No 22 from front.
- 4. Oakdene alongside No 20 from front.
- 5. Oakdene alongside No 22 from rear.
- 6. Oakdene alongside No 20 from rear.
- 7. No 22 projecting beyond Oakdene to rear.
- 8. No 20 at 7.500m separation from Flank wall of Oakdene.

2.4 WIDER SITE CONTEXT PHOTOGRAPHS



- 1. No 20 access driveway looking from rear looking towards Oaklands Road.
- 2. No 22 access driveway looking from rear looking towards Oakdene.
- 3. Kitchen window at ground floor to 20 Oaklands Road, note fencing in foreground.
- 4-8. Variety of properties along Oaklands Road
- 9. Other 1930's (possibly 1950's) [property on Oaklands Road]

3.0 PLANNING POLICY APPRAISAL

3.1 LB BROMLEY UDP RETAINED POLICIES
RESIDENTIAL EXTENSIONS

POLICY H8

The design and layout of proposals for the alteration or enlargement of residential properties will be required to satisfy all of the following criteria:

(i) the scale, form and materials of construction should respect or complement those of the host dwelling and be compatible with development in the surrounding area;

We consider the scale of the side extension to be appropriate, with the form and roof alterations designed to complement the host building.

Where an alternative form is adopted to the rear, this is to maintain the integrity of the original building, using a contemporary flat, parapet structure to distinguish itself from the 1930’s pitched roof form.

(ii) space or gaps between buildings should be respected or maintained where these contribute to the character of the area;

We have adopted the guidance of LB Bromley policy H9, regarding side space. There is additional space between the properties afforded by the driveways that serve both No’s 20 & 22 Oaklands Road, to either side of the site. This will maintain significant distance between the properties, even after extension. The nature of the tenure of these adjacent properties and the reliance on the car parking to the rear means that any side extension of these is extremely unlikely.

(iii) dormer windows should be of a size and design appropriate to the roofscape and sit away from prominent roof pitches, unless dormers are a feature of the area.

The proposed dormers are modest and sit on the rear facing slope. They also provide some relief to the extended roof form.

SIDE SPACE

POLICY H9

When considering applications for new residential development, including extensions, the Council will normally require the following:

(i) for a proposal of two or more storeys in height, a minimum 1 metre space from the side boundary of the site should be retained for the full height and length of the flank wall of the building; or

(ii) where higher standards of separation already exist within residential areas, proposals will be expected to provide a more generous side space. This will be the case on some corner properties.

See above (H8(ii)).

3.2 BROMLEY RESIDENTIAL DESIGN SPD

1.1 LOCAL DISTINCTIVENESS

The starting point for all new development should be a respect for the character and appearance of the site, its immediate neighbours and the wider street scene. Various types of character are defined in SPG1, and also in the Conservation Area SPG documents and in the descriptions contained in the UDP.

The character of Oaklands Road is wildly varied. The two adjacent properties are significant Victorian villas with an additional storey each of Victorian floor to ceiling height (approximately 3m higher than the proposed ridge height).

1.2 AMENITY

The provision of adequate space and light between buildings and impact on the privacy and amenity of adjoining properties are important factors that require careful consideration. The Council’s side space policy normally asks for a minimum 1m side space. However, it should be noted that in areas where there is a greater spatial quality more than 1m side space will be required.

See above (H8(ii)), additionally, the proposed ground floor extension sits behind the rear line of that of No 22. Where it projects beyond the rear of No 20 it is at a significant distance. While it sits fractionally beyond the 45° rule for site set out (BRE publication, Site Layout Planning For Daylight and Sunlight), this is at in excess of 13m distance from the affected window and is therefore considered acceptable (the threshold being 12m distance. It far exceeds the 25° vertical guidance. In addition the proposal is directly to the north of the adjoining window and in actual fact will have zero impact on the sunlight afforded this window. In any case this window serves a kitchen and is not for the purposes of BRE sunlight/ daylight analysis considered to be a habitable room.

Additionally the arrangement of the flats and driveway to the garages places a secondary fence between this window and the application site, which will block any view/ impact of the proposed ground floor extension.

1.3 SETTING AND LANDSCAPE

Every attempt should also be made to retain important elements on the site such as trees, established landscape and features of biodiversity interest. Of equal importance is the role gardens play in providing a setting in front of and around a building.

The creation of large areas of hardstanding rarely contributes to attractive front gardens. Amenity space should be both visually attractive and of some practical use to the residents. Where there is insufficient land to provide useful spaces the landscaping should be arranged to provide an attractive setting to the building, protect the privacy of residents, especially those living in ground floor accommodation and provide attractive edges and screening in keeping with the locality.

The application property has a significant garden (both front and rear). The proposed side extension sits on an unattractive side yard, while the rear extension still affords the occupants a 21m long rear garden. This is more than double the standard required in most Local Plans.



EXAMPLE OF PITCHED ROOF WITH GABLETS

1.4 DESIGN AND FORM

The character of the immediate locality should guide the built form and palette of materials. The use of quality materials and details on the front elevation of a building and inferior ones on the sides and rear is not normally acceptable. Similarly, the restriction of architectural treatments to only the front elevation should be avoided.

Many areas of Bromley have a strong established local distinctiveness. Careful study of their character can provide building forms and details that can help a scheme to enhance local character rather than harm it.

We believe that this proposal complies with this policy, with regard to the host property. The character of the local area is, however, extremely varied, so this aspect of the guidance is less applicable to this site.

1.5 ROOF AND DORMERS

The majority of buildings in Bromley have traditional pitched roof forms, contributing greatly to the streetscape and roofscape of their localities. Roof alterations should be carefully considered to ensure that they respect the form and appearance of the existing roof. Continuation of the roof line may be appropriate in some cases – in other cases it may be better to drop the roof line so that extensions are subservient to the host building.

Dormer windows are common in many parts of the borough and traditional dormers can often guide the appropriate location, form and details of new ones. Large or dominant dormers, or those which harm the over-all appearance of the building, should be avoided.

See H8(iii). We believe that this proposal complies with this policy.

While the original roof form to the side dropped in height from the height of the main crown, we have adopted the alternative strategy of raising this section of roof. Where there was once a crown roof, we have now created a ridge line with two side gablets.

The below example demonstrates this roof form, however in the case of this application roof will be set back from the front of the building and the main feature of the house, when viewed from the front, will remain the projecting two-storey bay, with a projecting hipped roof.

We also believe the height of the two adjacent buildings is a further mitigating factor.



EXAMPLE OF SINGLE STOREY FLAT ROOFED EXTENSION

1.6 GARAGES

Often new garages, especially those integrated into the house, are the most dominant feature on the site. However, the front door should be the primary element in the composition, treated as the focal point of the building facade.

Domestic garages should not be obtrusive and discordant elements in the street scene. Garages in the front gardens are rarely acceptable as they lead to a loss of garden space and harm the setting of the house. Ideally new garages should be located to the side or rear. Those to the side are best recessed behind the established building line and detailed in a manner that complements the detailing on the host building. Garage roofs, as those to extensions, should be given careful consideration.

The proposal does not retain the existing unusable (for a car) 1930’s garage, replacing it with a study and a bike store. These features are secondary to the front entrance in the same way that the existing garage is, with the front door recessed and served by a full width projecting step and a hipped roof continuing from the study/ bike store element. This clearly defines the front entrance.

1.7 PARKING

The Council is committed to reducing the dominance of cars and car parking on the public realm. Where opportunities present themselves, new parking should be provided at the side or rear of properties and separated by sufficient screening or distance from habitable rooms to protect residential amenity, orientation, and aspect.

Parking is as existing.

1.8 FENCES AND BOUNDARIES

Boundary treatments make an important contribution to the street scene. In most areas front boundaries rarely exceed 1m in height and in some suburban locations development is laid out without front boundaries to give a spacious open-plan appearance.

Fences and boundaries are as existing

2.2 EXTENSIONS

The rear elevation is usually the most appropriate location for extensions but careful design and detailing is still required. Where side extensions are deemed acceptable it is good practice to set them back from the façade of the building to give the original elevation due prominence. When considering proposals the Council will also give consideration to design, general amenity, impact on neighbours and potential loss of daylight and sunlight.

All extensions should respect the composition of the host building, especially the roof and the rhythm of form and detail generally. Particular care should be taken to retain the architectural integrity of the host building. For that reason very large extensions, which overwhelm their host buildings, are unlikely to be acceptable.

The side extension is only 1400mm, although the area over the garage is also infilled. This element is set back some 600mm from the front elevation, rendering the side elevation subservient to the main house.

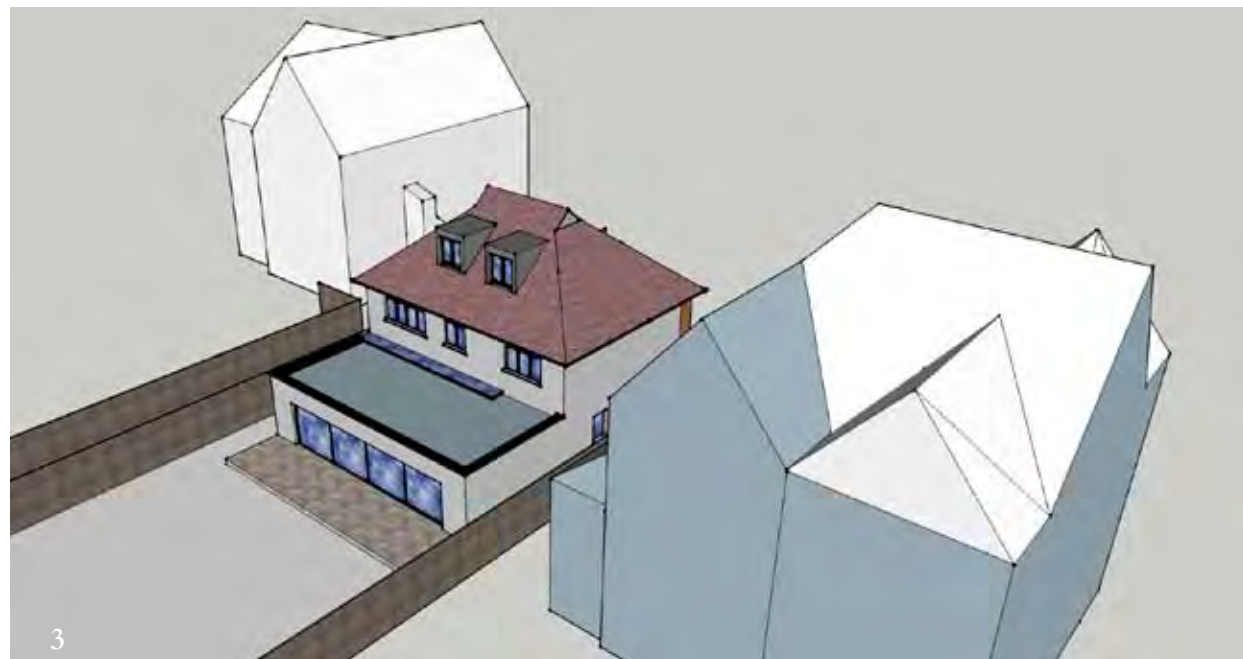
The design of the roof has been addressed in section 1.5 Roofs and Dormers.



FRONT ELEVATION FROM OAKLANDS ROAD



REAR ELEVATION FROM BACK GARDEN



1. View drim driveway to No 20 Oaklands Road
2. Wider aerial view from front.
3. Wider aerial view from rear.
4. Closer view of roof gablet and dormer windows to rear.
5. Approach view to Oakdene from along Oaklands Road. Note mature trees screen this view in reality.

5.0 ADDITIONAL CONSIDERATIONS

5.1 ACCESS

The site is well served by public transport with bus routes along Bromley Road, although Oaklands Road itself has a low PTAL rating of 1b.

All access arrangements are as existing.

As an extension, Part M of the building Regs, or Lifetime Homes requirements do not apply.

There is ample off-street parking on site, which is unaffected by the proposed extensions.

5.2 WASTE MANAGEMENT

Waste and recycling facilities will be as existing.

5.3 ENVIRONMENTAL STRATEGY

All work will be in compliance with the Building Regulations. Where Practical new fabric will exceed the requirements of the Building Regulations by 25%.

6.0 CONCLUSION

We believe that the context of the property on Oaklands Road makes this a suitable site for the proposed extension.

The proposals comply with the key policy for side extensions, Side Space H9, however they do involve the raising of the height of the existing roof, which currently drops down from the highest point, to the side wing adjacent to No 22 Oaklands Road.

This roof structure has been carefully designed and we feel does not adversely affect the appearance of the house.

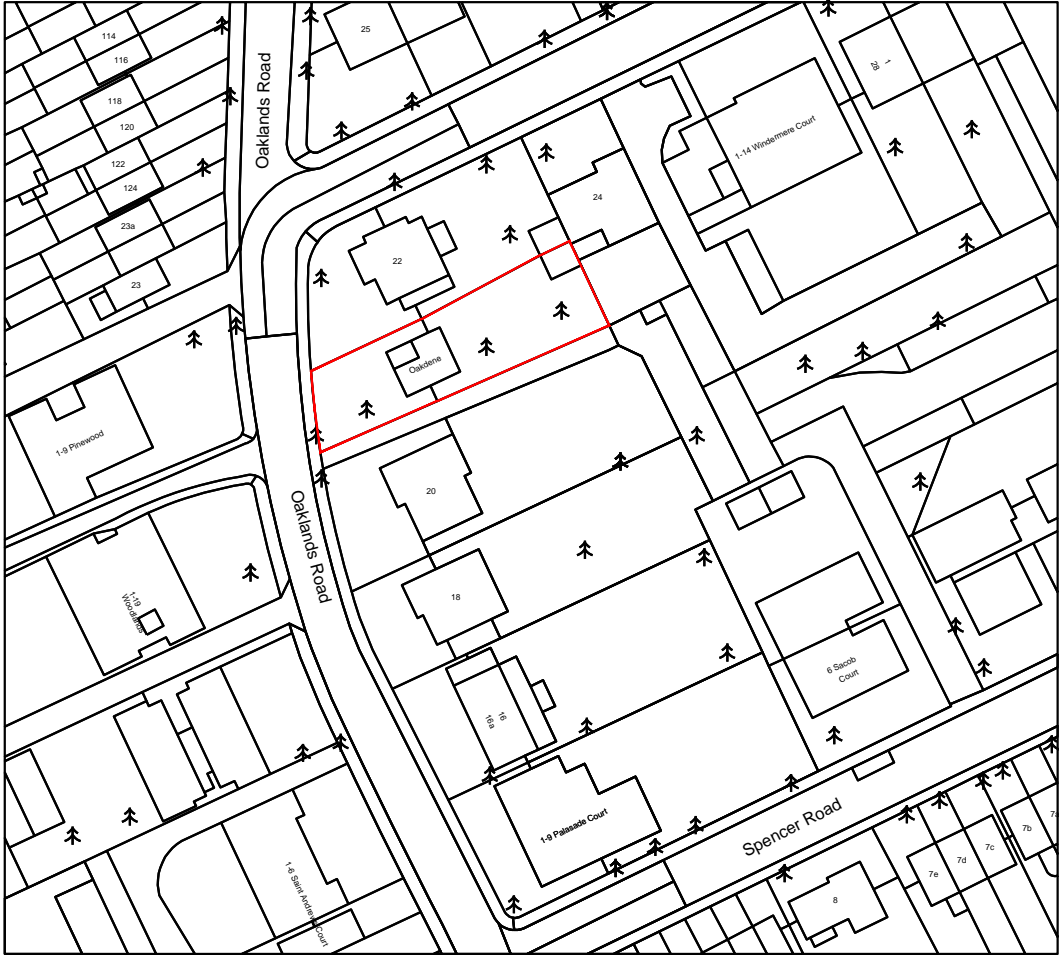
Oaklands Road is extremely varied and Oakdene is an individual house flanked by two far more massive Victorian Villas to either side This relationship would be unaffected by the proposals.

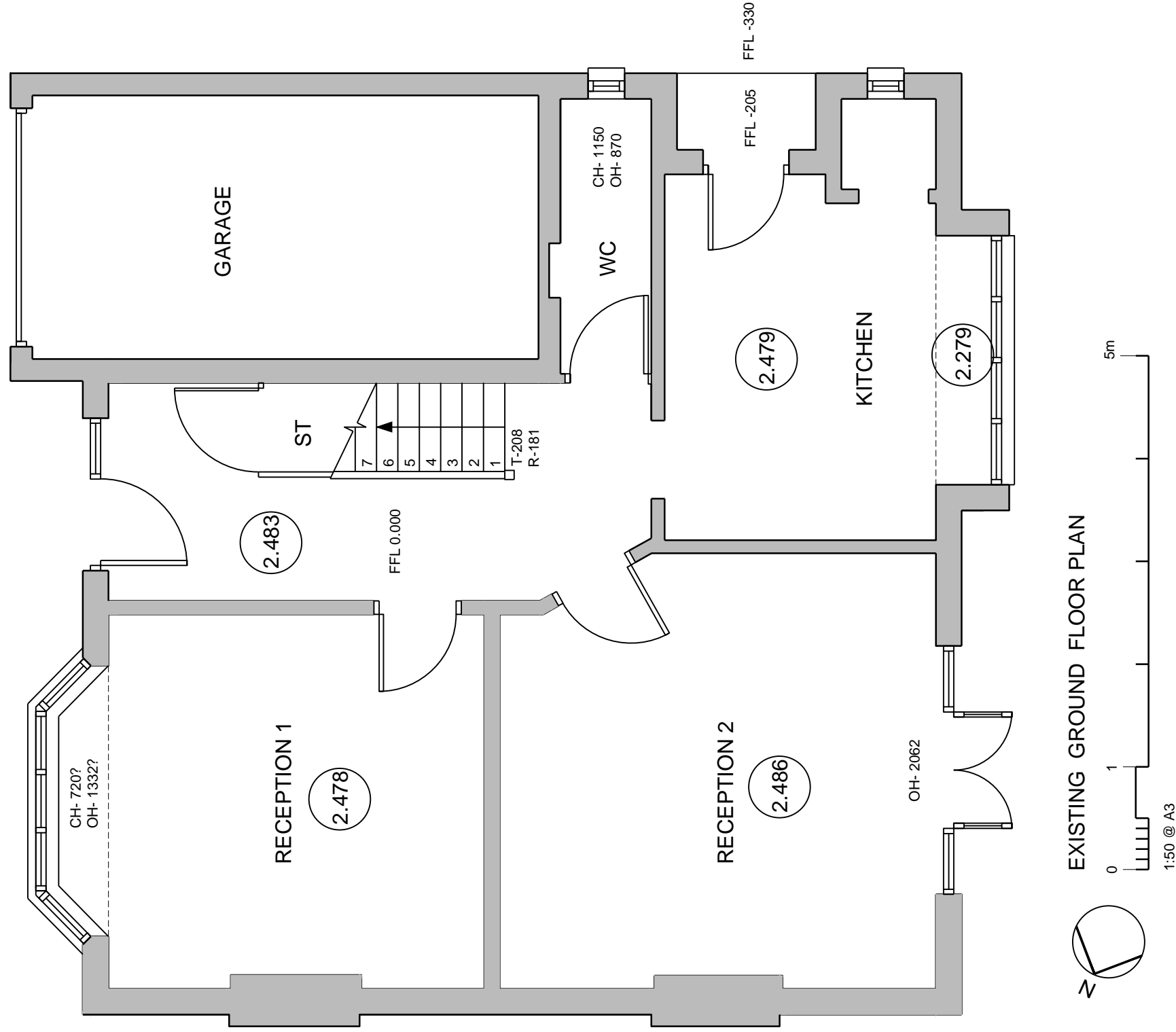
The front projecting bay window and the projecting ground floor structure (originally the garage, but proposed to be the study/ bike store) would remain the main architectural features in the foreground.

The rear extension has zero impact on the habitable rooms to the adjacent properties and will be at some considerable distance separated by existing driveways and screened by existing fence structures to both sides.

We feel that the proposal is coherent and respectful of the host property, notwithstanding the fact that it is not listed at any level and is outside any Conservation Area.

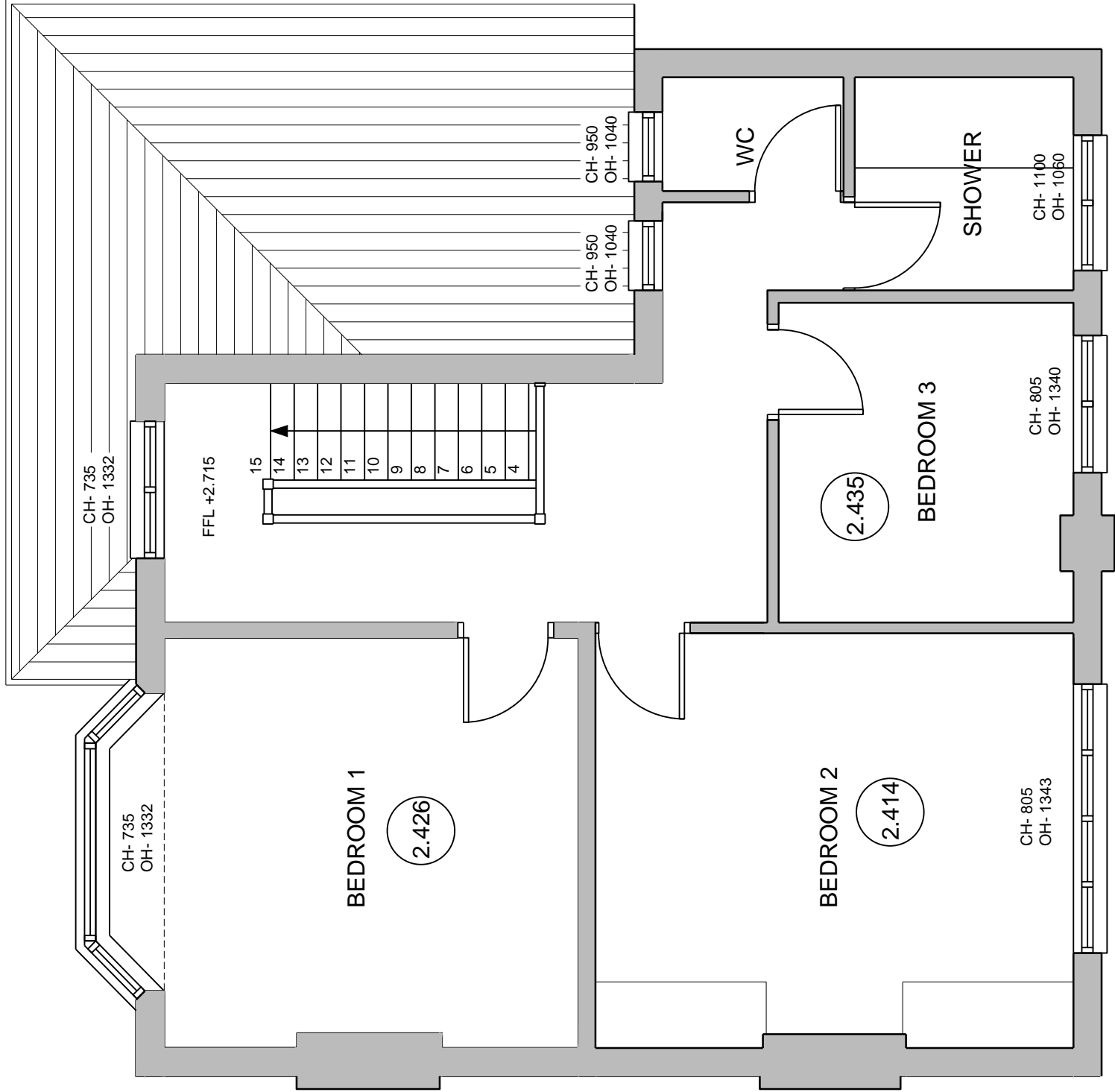
We trust that LB Bromley concurs with this and is minded to grant full planning consent.



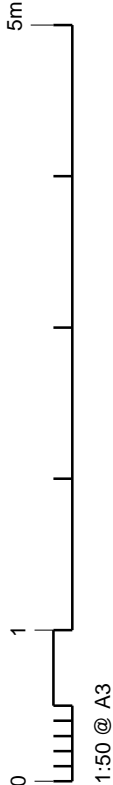


EXISTING GROUND FLOOR PLAN

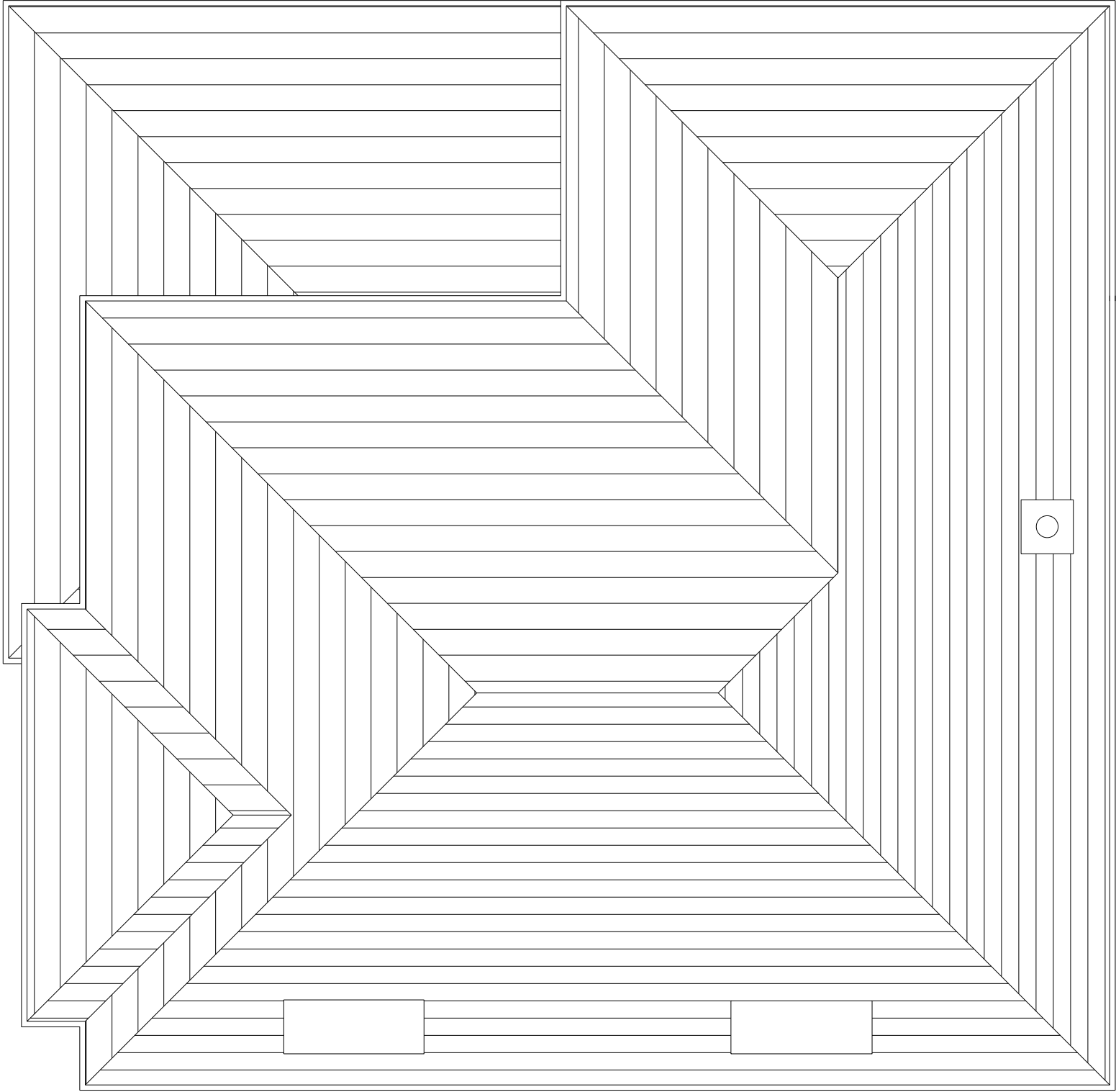
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			ALL DIMENSIONS TO BE CHECKED ON SITE BEFORE ANY WORK COMMENCES						



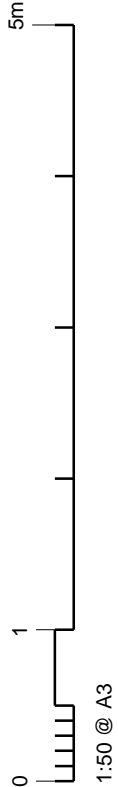
EXISTING FIRST FLOOR PLAN



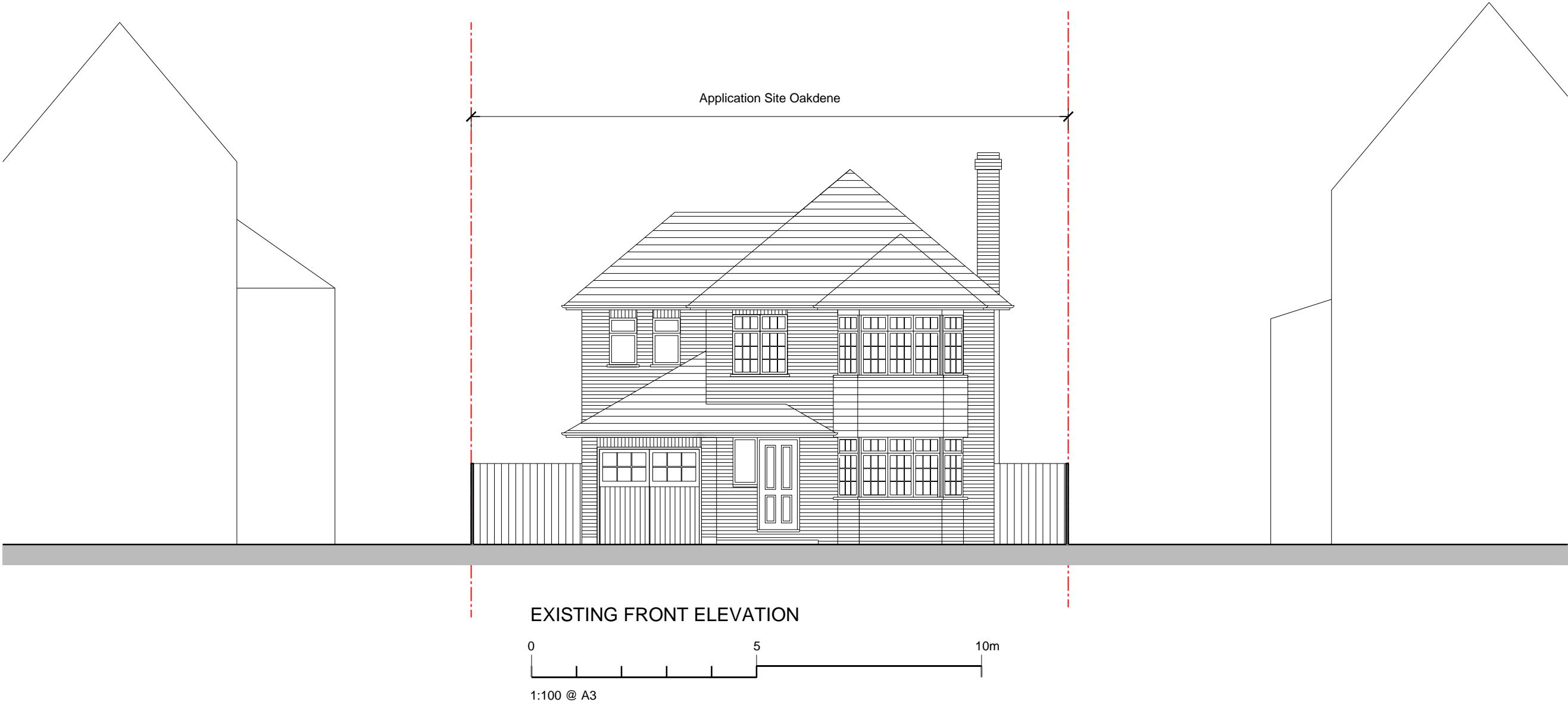
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							EX03		




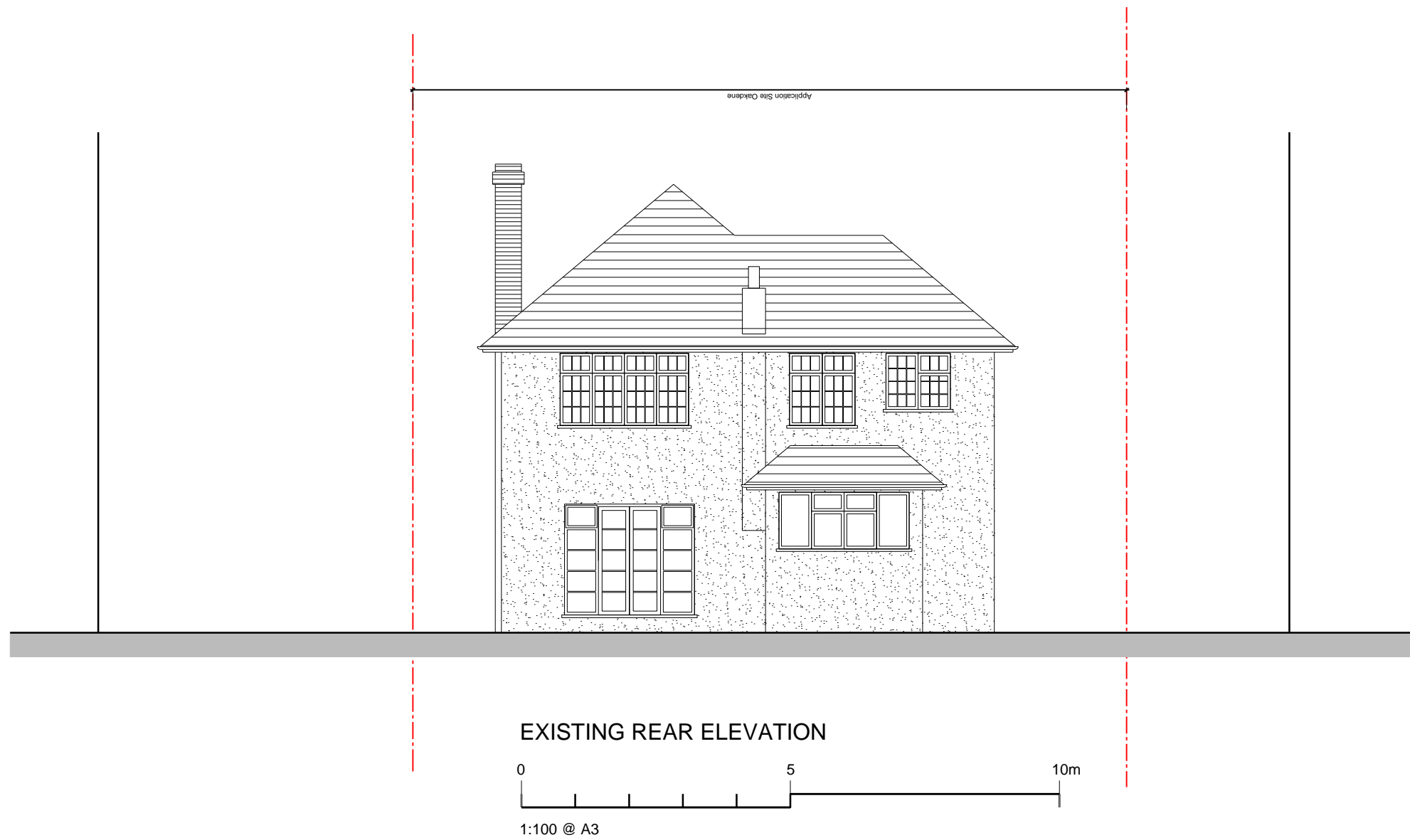
EXISTING ROOF PLAN



Rev	Description	Date	Notes	Job Title			Purpose of Issue		
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				Bromley, Kent, BR1 3SL			Revision		
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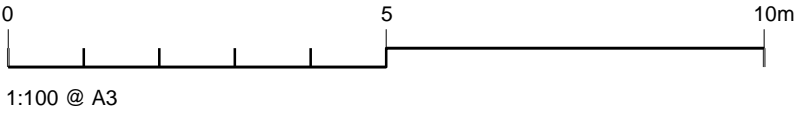
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				Front	Date		Drawing Number
				Project Address	24.03.2017		
				Oakdene, Oaklands Road	Revision		
				Bromley, Kent BR1 3SL	/	EX06	
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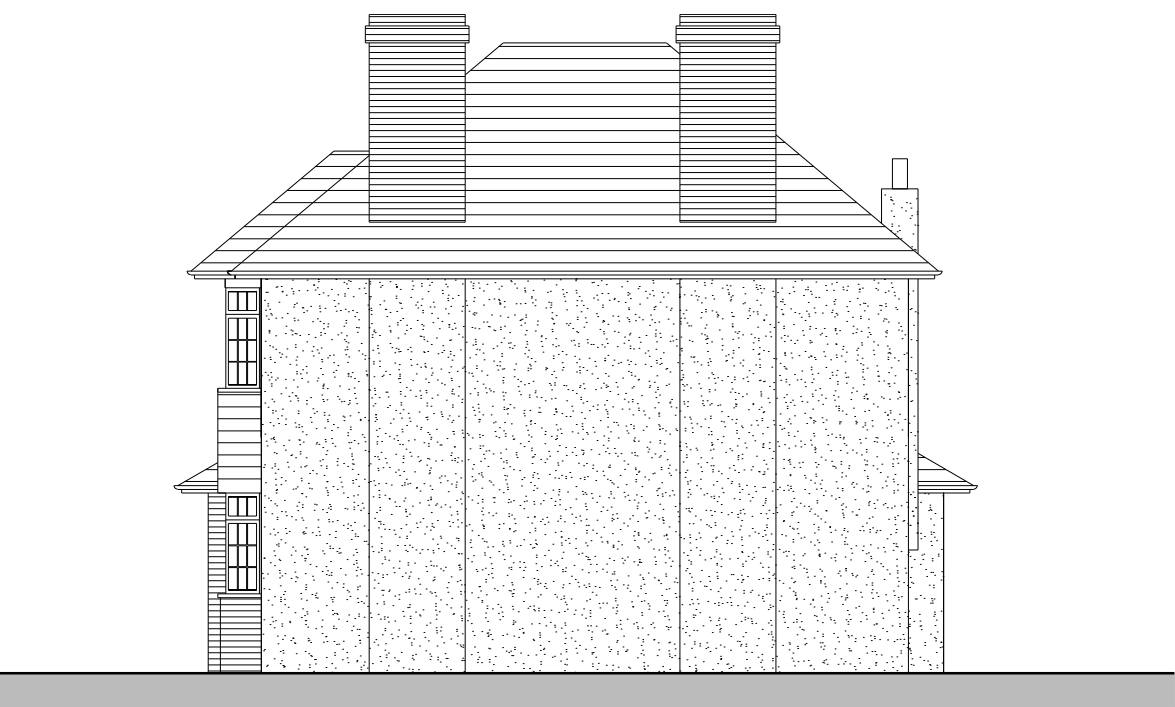
Rev	Description	Date	Notes	Job Title	Purpose of Issue	<div><div>NDA</div><div>Neel Daksby Architecture</div><div>43, Kellerton Road, London SE13 3RB +44 (0)7949 304 43 neel@neeldaksbyarchitecture.co.uk www.neeldaksbyarchitecture.co.uk</div></div>
				Abell House	Survey	
				Drawing Title	Job Number	
				Existing Elevations	1701	
				Rear	Drawing Number	
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				Bromley, Kent BR1 3SL	Revision	
					/	EX07



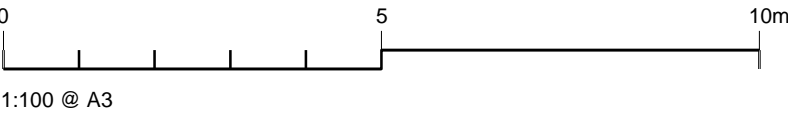
EXISTING NORTH ELEVATION



Rev	Description	Date	Notes	Job Title Abell House Drawing Title Existing Elevations North Project Address Oakdene, Oaklands Road Bromley, Kent BR1 3SL	Purpose of Issue Survey		<div><div>NDA</div><div>Neel Dakshy Architecture</div><div>43, Bellerton Road, London SE13 3RD +44 (0)7949 304 43 neel@neeldakshy.co.uk www.neeldakshy.co.uk</div></div>
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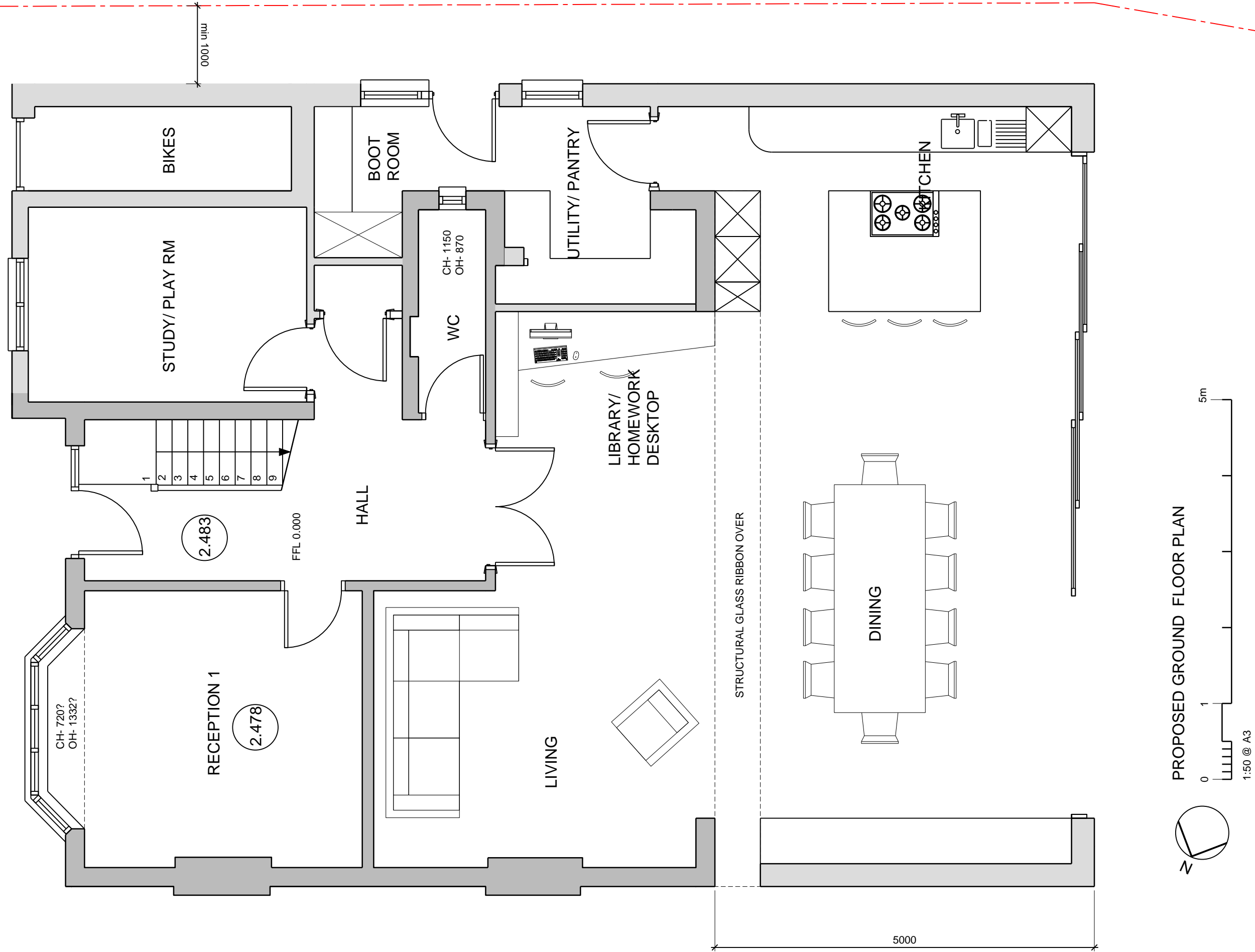
EXISTING SOUTH ELEVATION



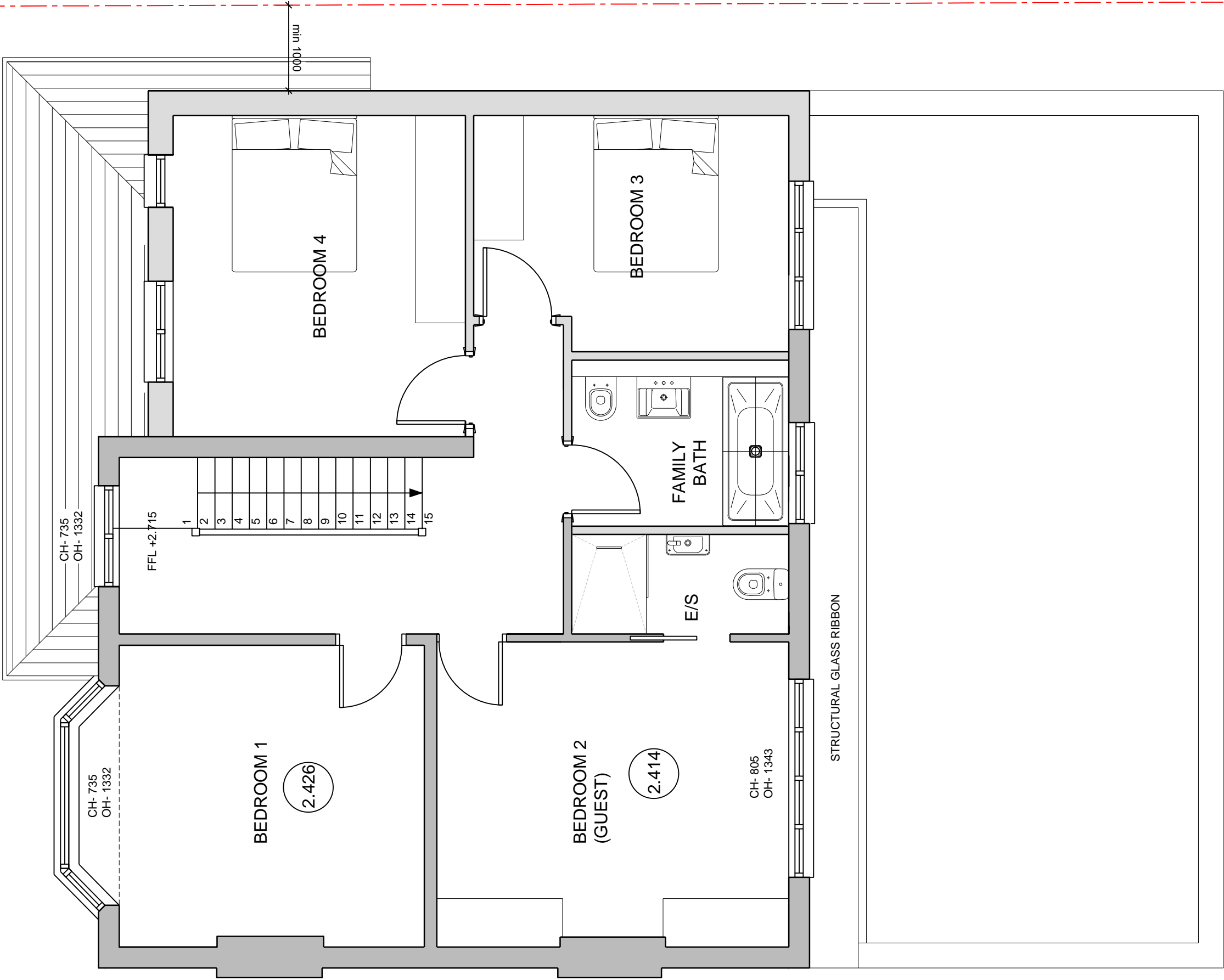
Rev	Description	Date	Notes	Job Title	Purpose of Issue	<div><div>NDA</div><div>Neel Dakshy Architecture</div><div>43, Kellerton Road, London SE13 3RD +44 (0)7949 304 43 neel@neelarchitecture.co.uk www.neelarchitecture.co.uk</div></div>	
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				South	Date		Drawing Number
				Project Address	24.03.2017	EX09	
				Oakdene, Oaklands Road	Revision		
				Bromley, Kent BR1 3SL	/		



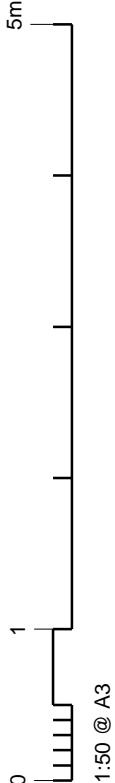
Rev	Description	Date	Notes	Job Title	Purpose of Issue		 43, Kellerton Road, London SE13 3RD +44 (0)7949 304 43 neel@neeldakshyarchitecture.co.uk www.neeldakshy.co.uk
				Abell House	Full Planning Application		
				Drawing Title	Scale @ A3	Job Number	
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Rev A	Description North point and scale bar included	Date 14.06.17	Notes <
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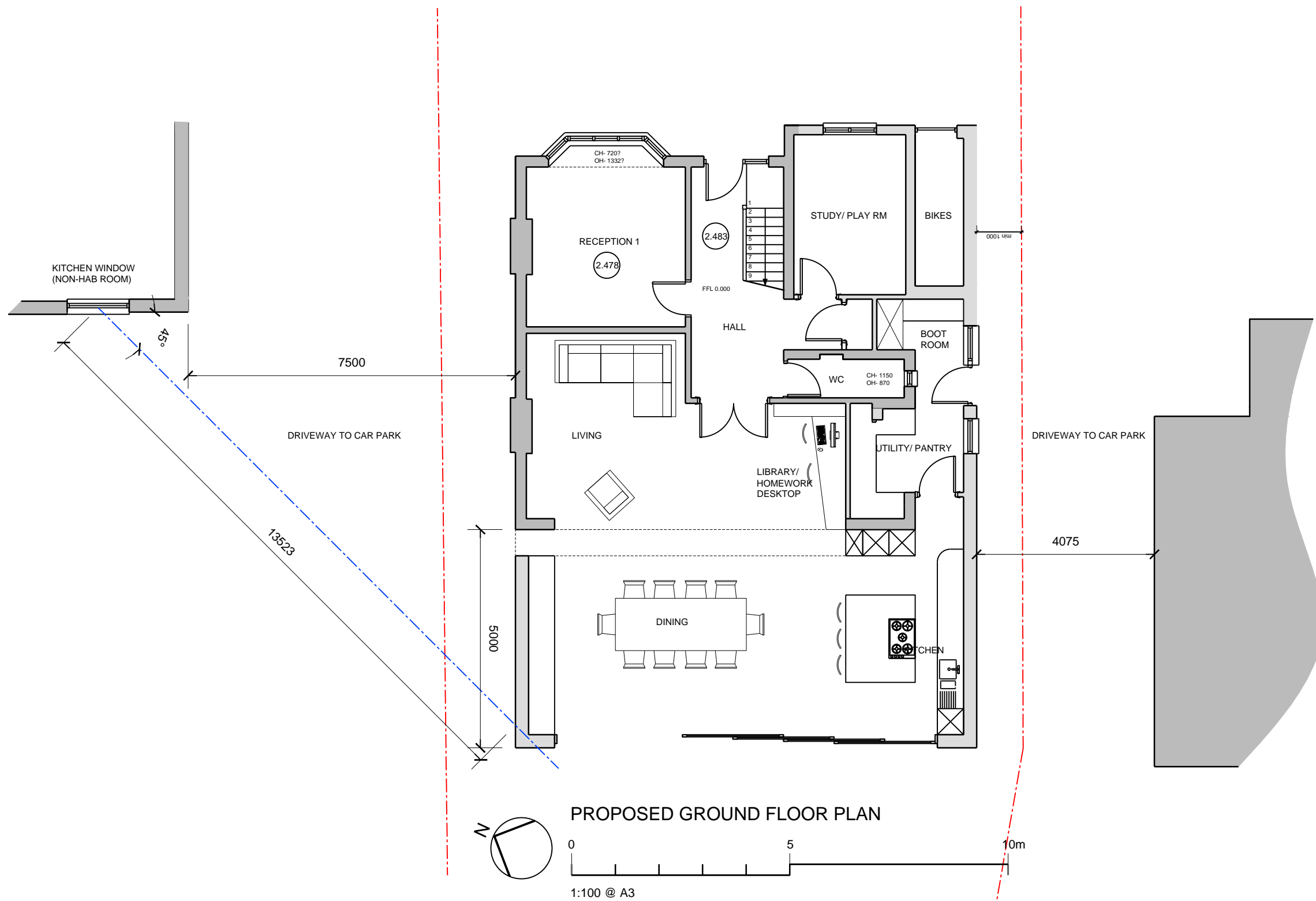
PROPOSED FIRST FLOOR PLAN




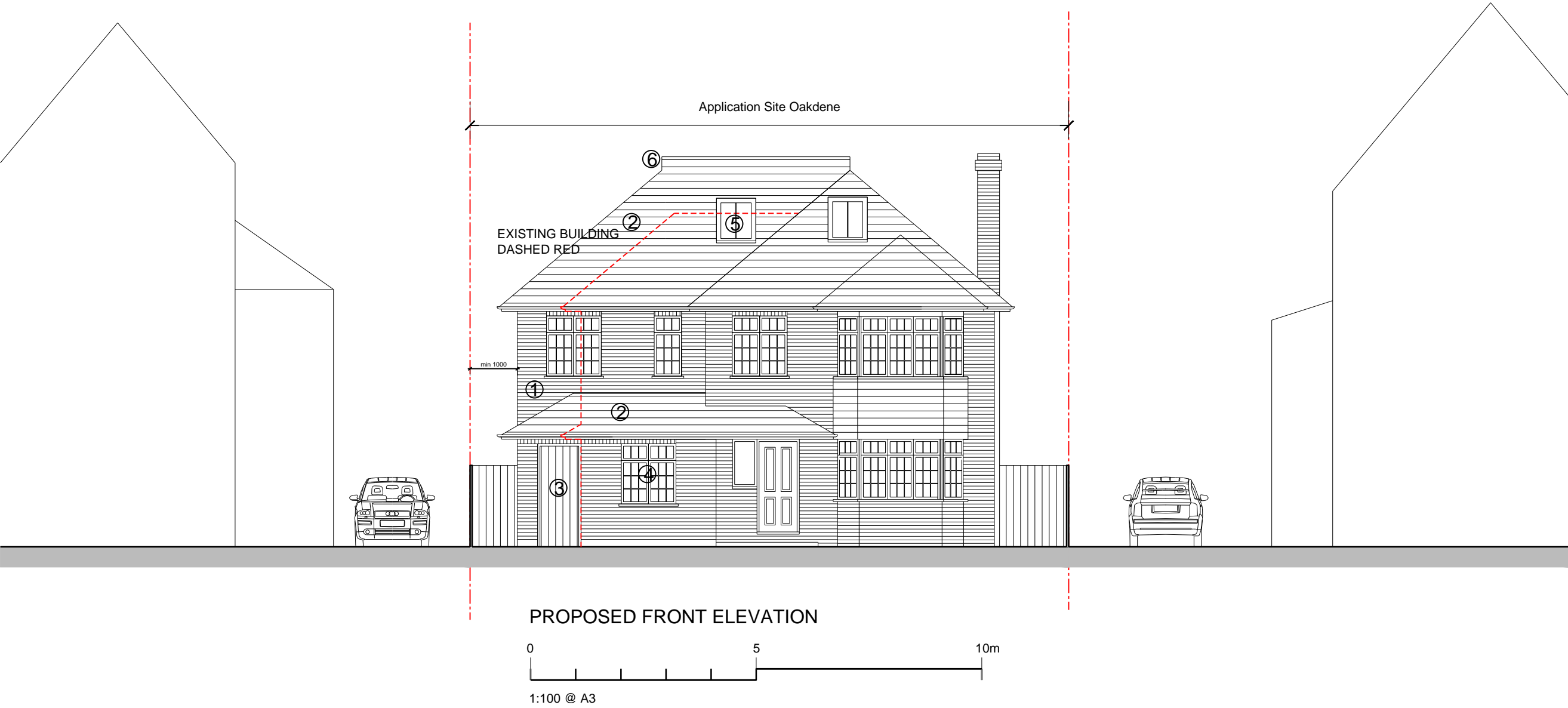
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								Revision /	



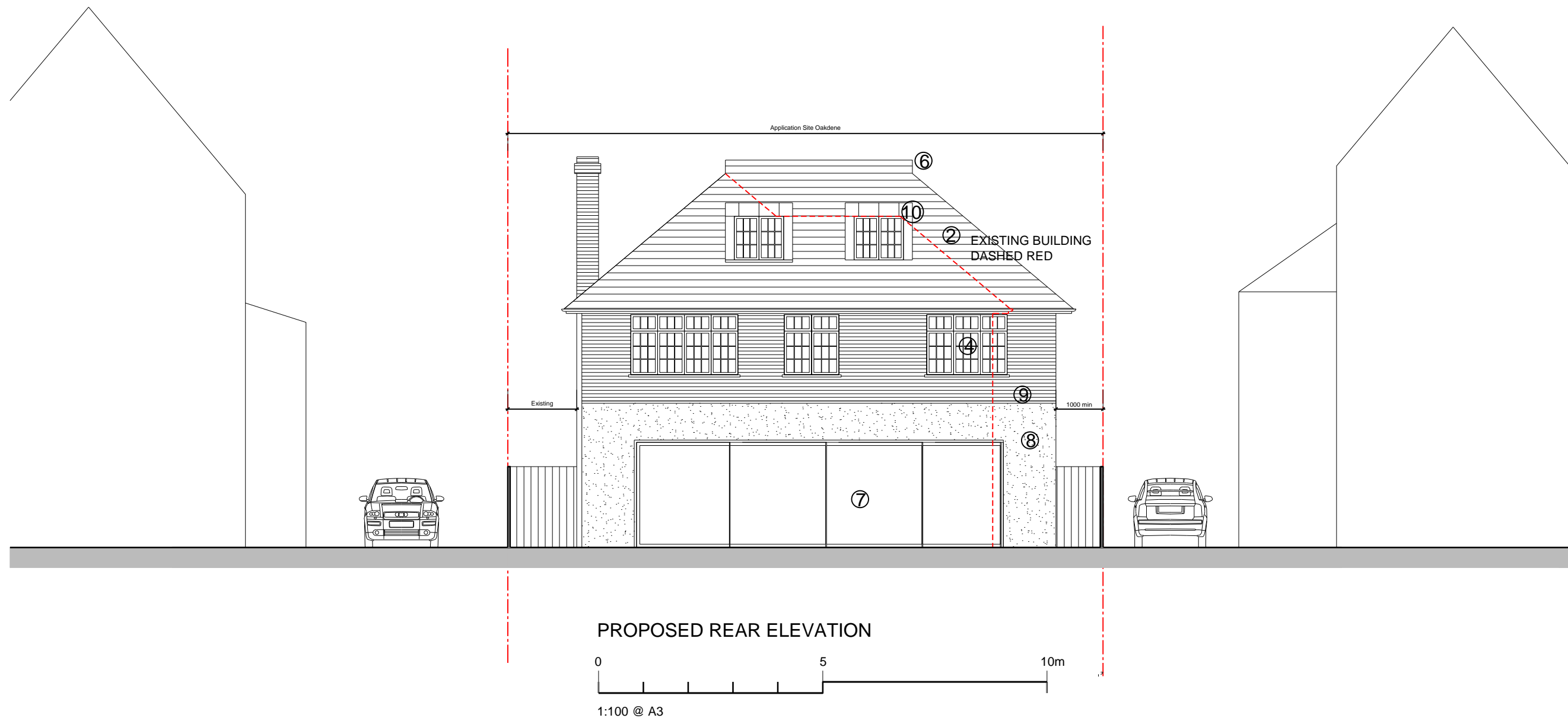
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Rev	Description	Date	Notes	Job Title	Purpose of Issue	Scale @ A3	Job Number	
A	Window to adjacent property labelled to reflect usage as non-habitable kitchen	14.06.17		Abell House	Full Planning Application	1:100@A3	1701	
				Drawing Title				
				Proposed Ground Floor				
				Relationship With Adjacent Properties				
				Project Address		Date		
				Oakdene, Oaklands Road		12.06.17		
				Bromley, Kent BR1 3SL		Revision		
						A		
							Drawing Number	
							PL06	



Rev	Description	Date	Notes				Job Title	Purpose of Issue		<div><div>NDA</div><div>Neel Dakshy Architecture</div><div>43, Bellerton Road, London SE13 3RB +44 (0)7949 304 43/ neel@neeldakshyarchitecture.co.uk www.neeldakshyarchitecture.co.uk</div></div>
			1	Brickwork to match existing	8	Painted sand cement render	Abell House	Scale @ A3	Job Number	
			2	Clay tiles to match existing with bonnet hip tiles	9	PPC Aluminium coping	Drawing Title	1:100@A3	1701	
			3	T&G solid timber door to store	10	Lead lined dormer windows	Proposed Front Elevation	Date		
			4	Double glazed casement windows with leaded lights to match existing	11	Structural glass ribbon rooflight linked to structural glass side window	Project Address	12.06.17	Drawing Number	
			5	Velux conservation rooflight with central mullion	12	Side barn door with upper glazed panel and lower T&G panel with side window	Oakdene, Oaklands Road	Revision		
			6	Existing roofslop extended past existing flat crown to form ridge	13	Double glazed casement windows	Bromley, Kent BR1 3SL	/		
			7	PPC Aluminium sliding doors						
			ALL DIMENSIONS TO BE CHECKED ON SITE BEFORE ANY WORK COMMENCES							

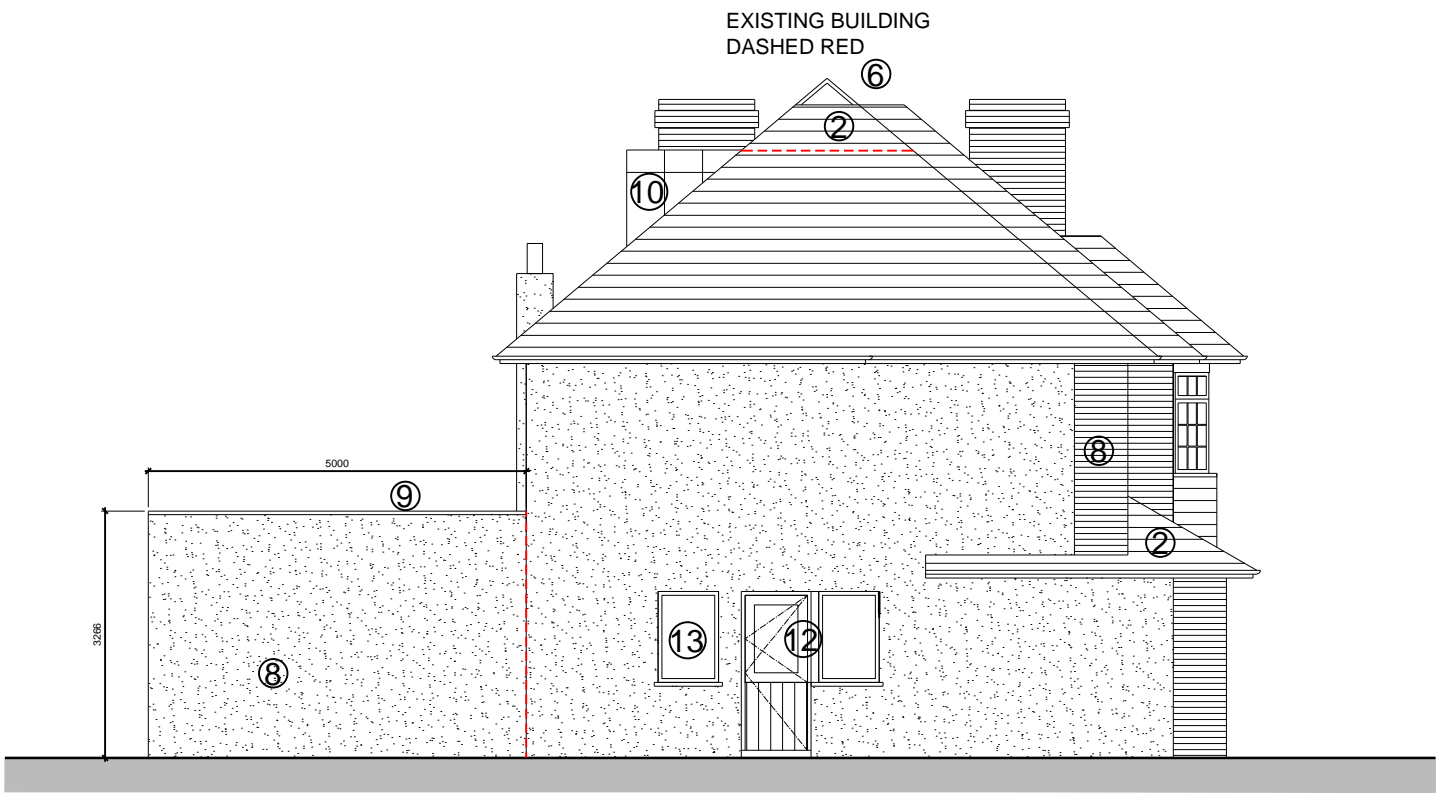


Rev	Description	Date	Notes		Job Title	Purpose of Issue		
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			2	Clay tiles to match existing with bonnet hip tiles	9	PPC Aluminium coping		
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			5	Velux conservation rooflight with central mullion	12	Side barn door with upper glazed panel and lower T&G panel with side window	Date	Drawing Number
			6	Existing roofslop extended past existing flat crown to form ridge	13	Double glazed casement windows	12.06.17	
			7	PPC Aluminium sliding doors			Revision	PL08
			ALL DIMENSIONS TO BE CHECKED ON SITE BEFORE ANY WORK COMMENCES			Project Address	/	
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						Drawing Title		
						Proposed Rear Elevation		
						Oakdene, Oaklands Road		
						Bromley, Kent BR1 3SL		

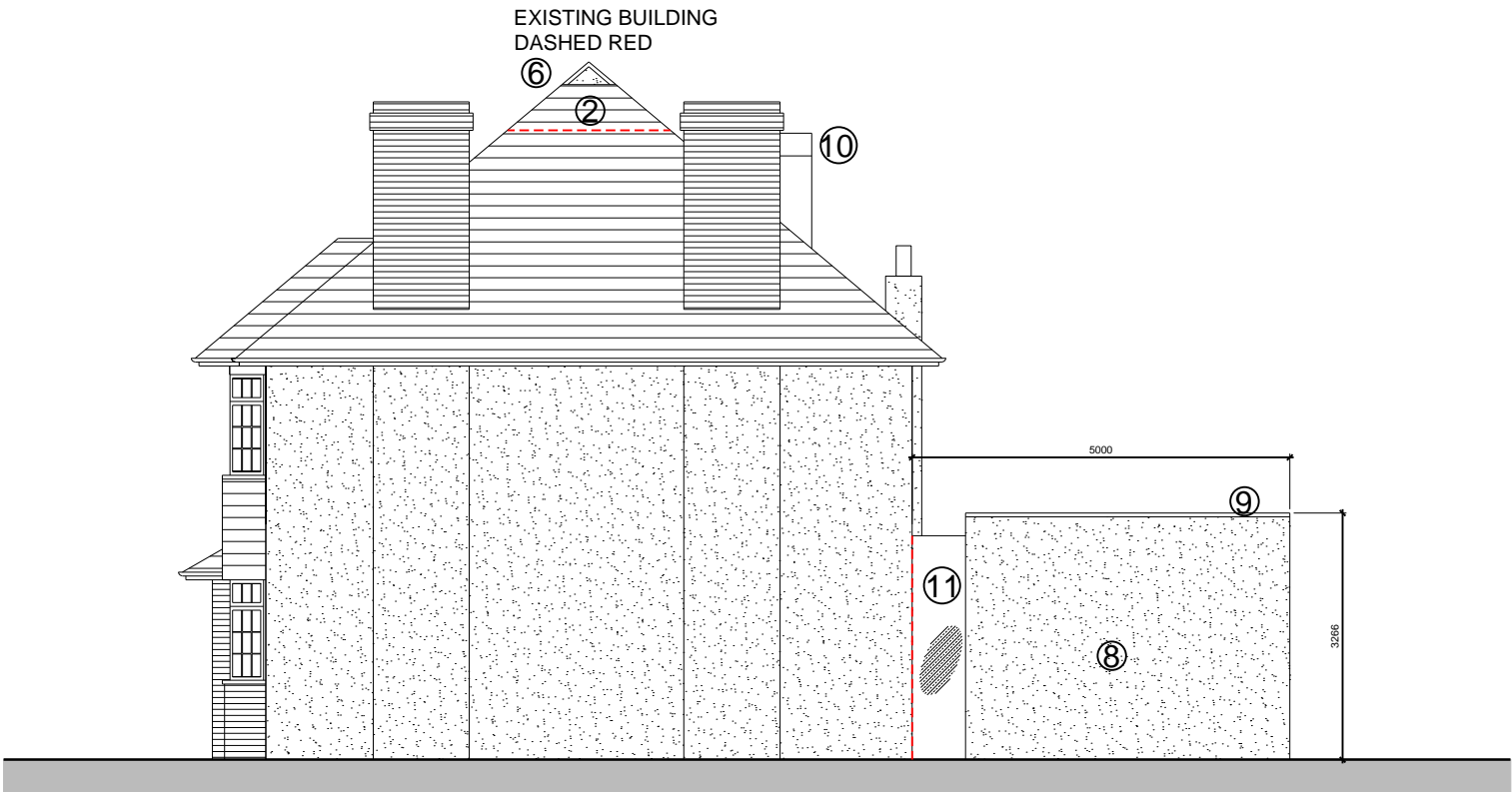
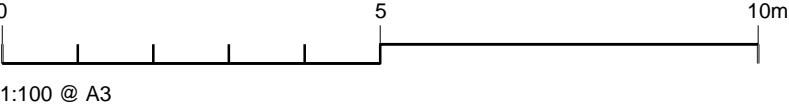
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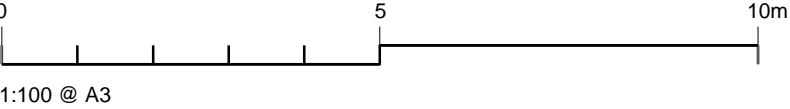
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EXISTING NORTH ELEVATION



EXISTING SOUTH ELEVATION



Rev	Description	Date	Notes		Job Title	Purpose of Issue		
			1	Brickwork to match existing	8	Painted sand cement render	Full Planning Application	
			2	Clay tiles to match existing with bonnet hip tiles	9	PPC Aluminium coping		
			3	T&G solid timber door to store	10	Lead lined dormer windows	Scale @ A3	Job Number
			4	Double glazed casement windows with leaded lights to match existing	11	Structural glass ribbon rooflight linked to structural glass side window	1:100@A3	1701
			5	Velux conservation rooflight with central mullion	12	Side barn door with upper glazed panel and lower T&G panel with side window	Date	Drawing Number
			6	Existing roofslop extended past existing flat crown to form ridge	13	Double glazed casement windows	12.06.17	
			7	PPC Aluminium sliding doors			Revision	PL09
			ALL DIMENSIONS TO BE CHECKED ON SITE BEFORE ANY WORK COMMENCES				/	

